

VICINITY MAP

PROJECT DATA

SITE ADDRESS: 4027 93RD AVE SE, MERCER ISLAND, WA 98040 PARCEL: 003100-0095 PROPERTY OWNER: SAM CHUNG PHONE NUMBER: 415-202-3226 EMAIL: KATECHUNGHOMES@GMAIL.COM APPLICANT:STEVE JOO PHONE NUMBER:206-306-6738 EMAIL:STEVE.JOO@HOTMAIL.COM

LOT SIZE: 8,575 SQ. FT.

LEGAL DESCRIPTION: ACERSON PARK ADD 19 LESS N 28 FT & 42 FT OF 20 PLat Block: A Plat Lot: 19-20

LOT ZONING: R-8.4 MAXIMUM LOT COVERAGE(BUILDING AND DRIVING SURFACES): 40% (LOT SLOPE LESS THAN 15%) MAXIMUM GROSS FLOOR AREA: 40% MAXIMUM BUILDING HEIGHT: 30 FEET PROPOSED PARKING: 1 COVERED AND 1 UNCOVERED 1 COVERED STALL IS REQUIRED (UNDER 3,000 SQ. FT. GFA)

GROSS FLOOR AREA

LOT AREA	8,575 SQ. FT.
EXISTING FINISHED AREA	1,630 SQ. FT.
EXISTING GARAGE AREA CONVERGED TO LIVING AREA	480 SQ. FT.
PROPOSED ADDITION (DETACHTED GARAGE)	318 SQ. FT.
NEW GROSS FLOOR AREA	2,428 SQ. FT.
2,428 / 8,575 X 100 = 28.3%	

LOT COVERAGE

EXISTING (HOUSE BUILT IN YEAR 1954)	
ALL ROOVES(INCLUDING EAVES)	2,619 SQ. FT.
DRIVEWAY(ASPHALT)	893 SQ. FT.
COVERED PATIO(CONCRETE)	0 SQ. FT.
TOTAL	3,512 SQ. FT.
3,512 / 8,575 X 100 = 40.96% (HOUSE BUILT IN	NYEAR 1954)
PROPOSED	
ALL ROOVES(INCLUDING EAVES)	2,619 SQ. FT.
DRIVEWAY(ASPHALT)	465 SQ. FT.
DETACHED GARAGE ROOF WITHIN EXISTING DRIVEWAY FOOTPRINT	356 SQ. FT.
COVERED PATIO(CONCRETE)	0 SQ. FT.
TOTAL	3,440 SQ. FT.
3,440 / 8,575 X 100 = 40.12%	
NO NEW LOT COVERAGE IS PROPOSED. PRO GARAGE IS IN THE SAME FOOTPRINT OF THE	

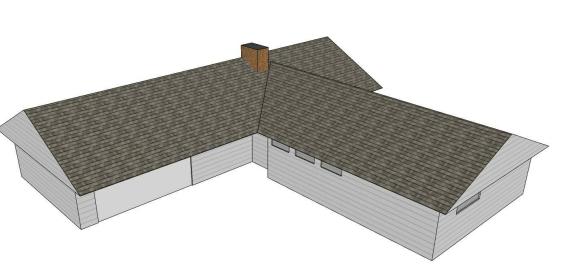
HVAC, PLUMBING, ELECTRICAL ARE UNDER SEPARATE PERMITS

AVERAGE BUILDING ELEVATION PROPOSED DETACHED GARAGE

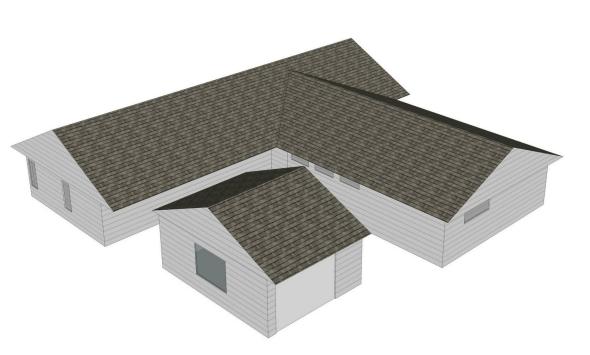
MIDPOINT ELEVATION	WALL SEGMEN
J = 312.0 feet	j = 15.99 fe
K = 312.5 feet	k = 20.00 f
M = 312.7 feet	m = 15.99
N = 312.3 feet	n = 20.00 f
ARE -((312 0)/15 00)+(31	2 5\(20 00\+(312 7

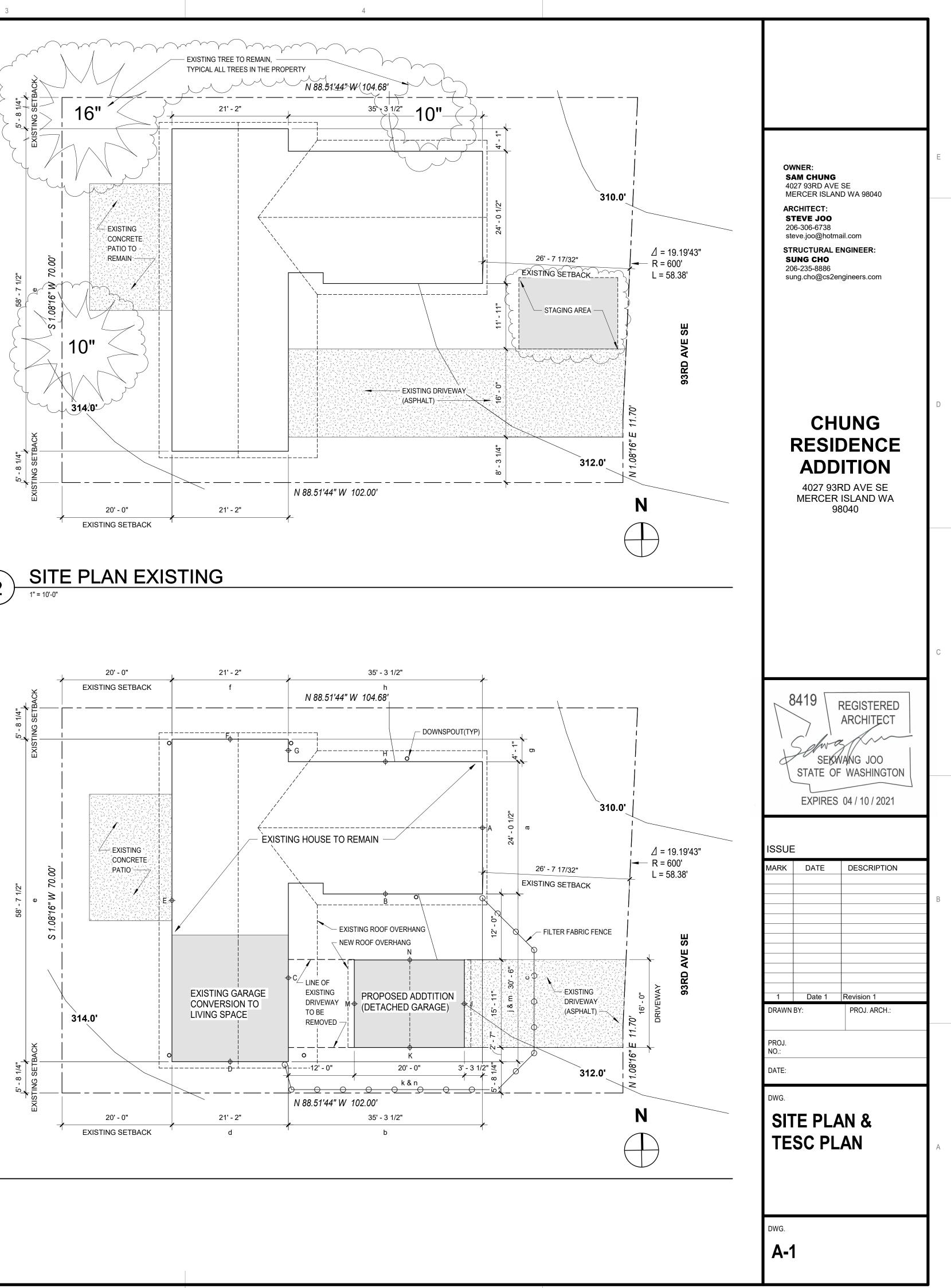
ABE =((312.0)(15.99)+(312.5)(20.00)+(312.7 (312.3)(20.00) / (15.99+20.00+15.99+20.00)

= 22,484.95 / 71.98 = 312.38 feet



EXISTING HOUSE





PROPOSED ADDITION



LINETYPE LEGEND PROPERTY LINE _____ FILTER FABRIC FENCE EXISTING GRADE LINE —312.0'—

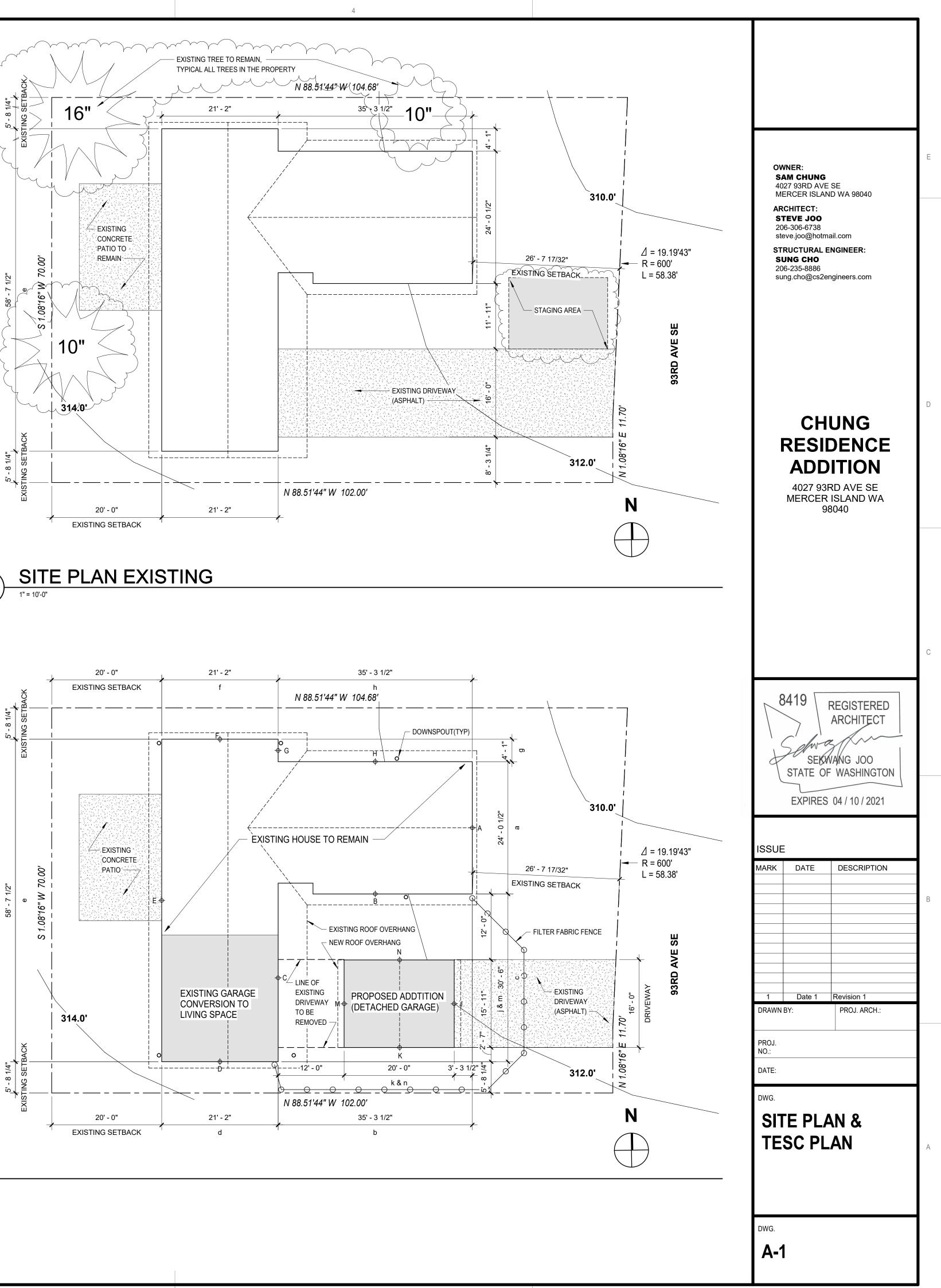
<u>LEVATION 2 (ABE2)</u> D GARAGE	AVERAGE BUILDIN EXISTING HOUSE	<u>G ELEVATION 1 (ABE1)</u>
LL SEGMENT LENGTH	MIDPOINT ELEVATION	WALL SEGMENT LENGTH
j = 15.99 feet	A = 311.5 feet	a = 24.04 feet
k = 20.00 feet	B = 312.4 feet	b = 35.29 feet
m = 15.99 feet	C = 313.0 feet	c = 30.50 feet
n = 20.00 feet	D = 313.6 feet	d = 21.16 feet
11 - 20.00 leet	E = 313.3 feet	e = 58.63 feet
(20.00)+(312.7)(15.99)+	F = 312.7 feet	f = 21.16 feet
.00)	G = 312.5 feet	g = 4.08 feet
.00)	H = 312.2 feet	h = 35.29 feet
		2.4)(35.29)+(313.0)(30.50)+ 3.3)(58.63)+(312.7)(21.16)+ 2)(35.29)) /

35.29)

= 312.72 feet

1

= 71,973.38 / 230.15



SITE PLAN PROPOSED

(24.04+35.29+30.50+21.16+58.63+21.16+4.08+

NOTE:

THE CONSTRUCTION DOCUMENTS REPRESENTED HEREIN ARE BASED ON NON-INVASIVE SITE OBSERVATION OF THE EXISTING STRUCTURAL CONDITIONS OF THE PROPOSED PROJECT. AS A RESULT, IN CERTAIN INSTANCES, DESIGN ASSUMPTIONS WERE USED TO FORMULATE THE COMPATIBILITY OF THE NEW CONSTRUCTION WITH THE EXISTING STRUCTURAL ELEMENTS. DURING THE COURSE OF CONSTRUCTION, IT IS POSSIBLE THAT CONDITIONS MAY BE ENCOUNTERED THAT DO NOT COINCIDE WITH THE DESIGN ASSUMPTIONS AND MAY REQUIRE FURTHER STRUCTURAL REVIEW TO DETERMINE ADEQUACY. THE BUILDER SHALL BE OBSERVANT OF THESE CONDITIONS AND IMMEDIATELY REPORT ANY DISCREPANCIES TO THE ARCHITECT PRIOR TO PROCEEDING FURTHER WITH THE WORK.

GENERAL NOTES

TRESTED OR CEDAR

- VERIFY IN THE FIELD ALL DIMENSIONS OF EXISTING CONDITION

- CONSTRUCTION TO COMPLY WITH THE 2015 IRC

- NEW 2X6 EXTERIOR STUD WALLS @ 24" O.C. STACKED FRAMED - NEW 2X4 INTERIOR STUD WALLS @ 16" O.C. UNLESS NOTED OTHERWISE - BEARING AND HEADERS TO BE 6X8 #2 DF OR 4X10 #2 DF FURRED-OUT

- FIREBLOCK ALL PLUMBING PENETRATIONS - PROVIDE SOLID BLOCKING OVER BEAMS & BEARING WALLS

- ALL HANGER TO BE SIMPSON OR EQUAL

- BEDROOMS TO HAVE AT LEAST ONE OPERABLE WINDOW NET CLEAR OPENING OF 5.7 SQ. FT. MIN. WITH A NET CLEAR OPENING HT. OF 24" MIN. AND NET CLEAR OPENING WIDTH OF 20" MIN. AND A FINISHED SILL HEIGHT

OF NOT MORE THAN 44" ABOVE FINISH FLOOR. - FIREPLACES AND STOVES MUST BE D.O.E. APPROVED AND BE TESTED, CERTIFIED & LABELED AS SUITABLE FOR USE DURING A FIRST STAGE BURN

BAN - ALL LUMBER EXPOSED TO WEATHER OR CONCRETE TO BE PRESSURE

<u>PLAN LEG</u>	END
	NEW CONSTRUCTION
	EXISTING WALL TO REMAIN
	EXISTING WALLS AND WINDOWS TO BE REMOVED AND OR MODIFIED
D-#	

NEW DOOR

EXISTING DOOR

t======

EXISTING DOOR TO BE REMOVED AND OR MODIFIED ā

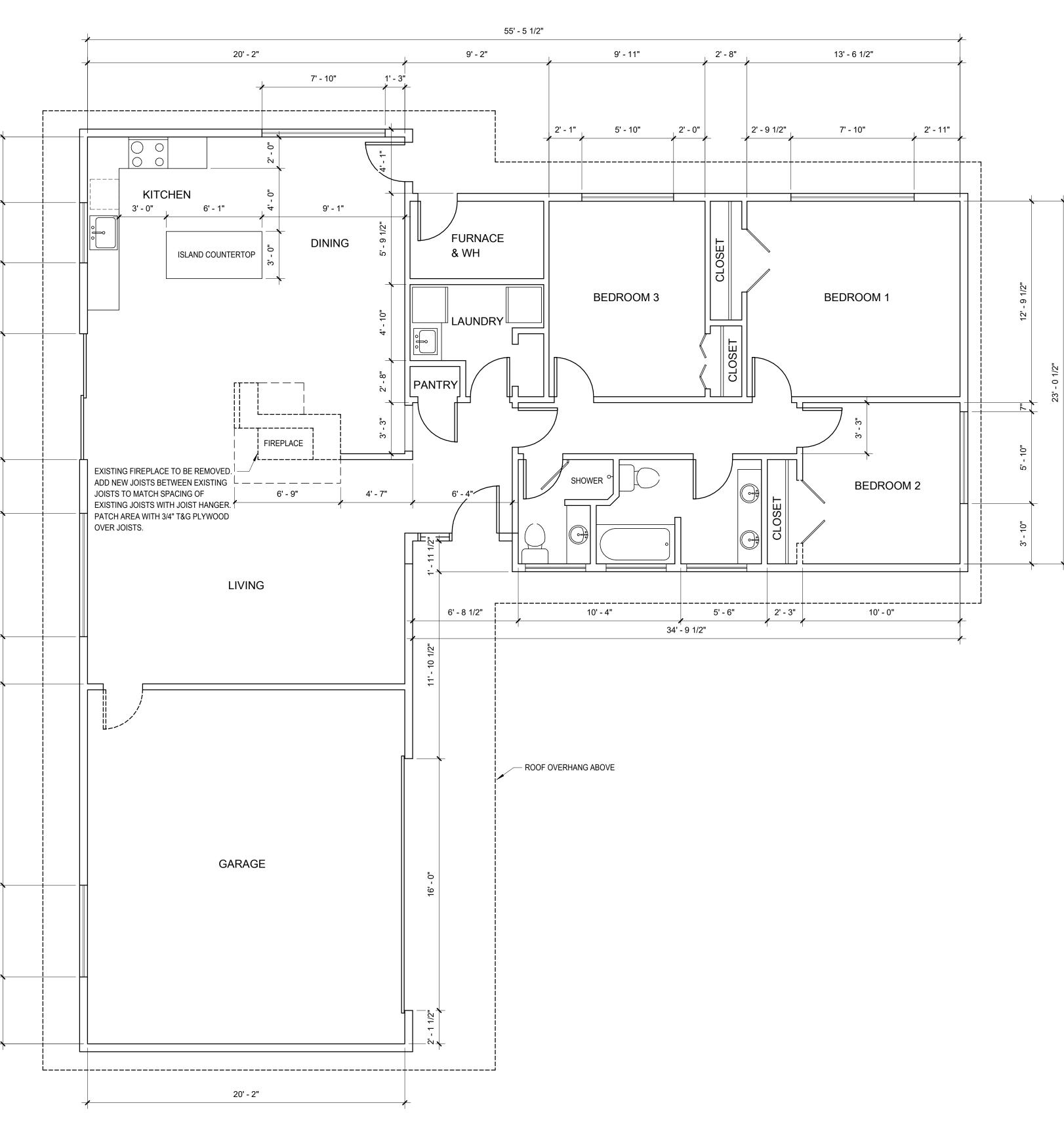
o

4' - 3"

2

1) EXISTING FLOOR PLAN

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0' 2' 4' 8'

3



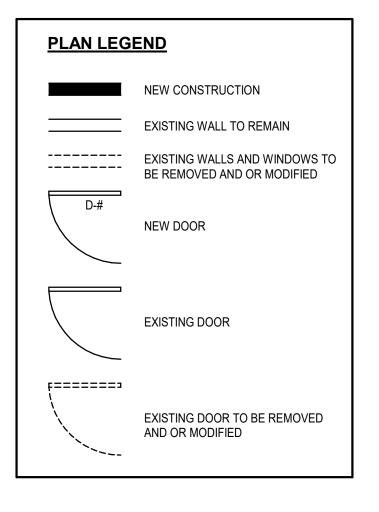
OWNER: SAM CHUNG 4027 93RD AVE SE MERCER ISLAND WA 98040 ARCHITECT: **STEVE JOO** 206-306-6738 steve.joo@hotmail.com STRUCTURAL ENGINEER: SUNG CHO 206-235-8886 sung.cho@cs2engineers.com CHUNG RESIDENCE ADDITION 4027 93RD AVE SE MERCER ISLAND WA 98040 8419 REGISTERED ARCHITECT SEKWANG JOO STATE OF WASHINGTON EXPIRES 04 / 10 / 2021 ISSUE MARK DATE DESCRIPTION DRAWN BY: PROJ. ARCH.: PROJ. NO.: 12/17/2016 DATE: DWG. **EXISTING FLOOR** PLAN DWG. **A-2**

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- ALL LUMBER EXPOSED TO WEATHER OR CONCRETE TO BE PRESSURE TRESTED OR CEDAR



SQUARE FOOTAGE

	LIVING AREA	KISTING		PROPOSED											
	LIVING AREA	GARAGE	SUBTOTAL	LIVING AREA	GARAGE	SUBTOTAL									
1ST FLOOR	1,630	480	2,110	2,110	318	2,428									

SHEAR WALL SCHEDULE

MARK	SHEATHING COMMON	STUDS AT ABUTTING PANEL	PANEL EDGE NAILING	RIM JOIST OR E		2X BOTTOM PLATE ATTACHMENT	SILL PLATE A	TTACHMENT
		EDGES*	**/****	SOLID RIM	TJI RIM	BOTTOM PLATE TO RIM JOIST BELOW**	ANCHOR BOLT TO CONCRETE ***	SILL PLATE AT FOUND.
SW1	7/16" OSB ONE SIDE	2X	8d @ 6" O.C.	A35 @ 24" O.C.	16d @ 6" O.C.	16d @ 6" O.C.	5/8" DIA. @ 48" O.C.	2X

* INTERMEDIATE FRAMING TO BE 2x STUDS. TWO 2x STUDS MAY BE USED IN PLACE OF 3x STUDS; NAIL TOGETHER WITH BOTTOM PLATE ATTACHMENT NAILING.

** 8d NAILS SHALL BE 0.131" DIA X 2 1/2" (COMMON). 16d NAILS SHALL BE 0.135" DIA X 3 1/2" (BOX). *** USE ANCHOR BOLT WITH 7" MIN. EMBED OR EXPANSION BOLTS WITH 4" EMBED. AT ALL ANCHOR BOLTS USE 3"X3"X1/4" PLATE WASHERS (SIMPSON BP5/8-3 OR EQUAL)

**** TWO STUDS MINIMUM ARE REQUIRED AT EACH END OF SHEARWALLS. END STUD SHALL RECEIVE PANEL EDGE NAILING.

WSEC - ENERGY EFFICIENCY CREDITS

SELECTED OPTIONS	DESCRIPTION	CREDIT
EFFICIENT BUILDING ENVELOPE 1a:	VERTICAL PENESTRATION U=0.28, FLOOR R=38	0.5
HIGH EFFICIENCY HVAC EQUIPMENT 3a:	GAS FURNACE W/ MINIMUM AFUE OF 94%	1.0
EFFICIENT WATER HEATING 5a:	KITCHEN SINK FAUCET AND SHOWERHEAD AT 1.75 GPM OR LESS	0.5
EFFICIENT WATER HEATING 5c:	GAS WATER HEATER W/ MINIMUM EF 0.91	1.5

WHOLE HOUSE FAN

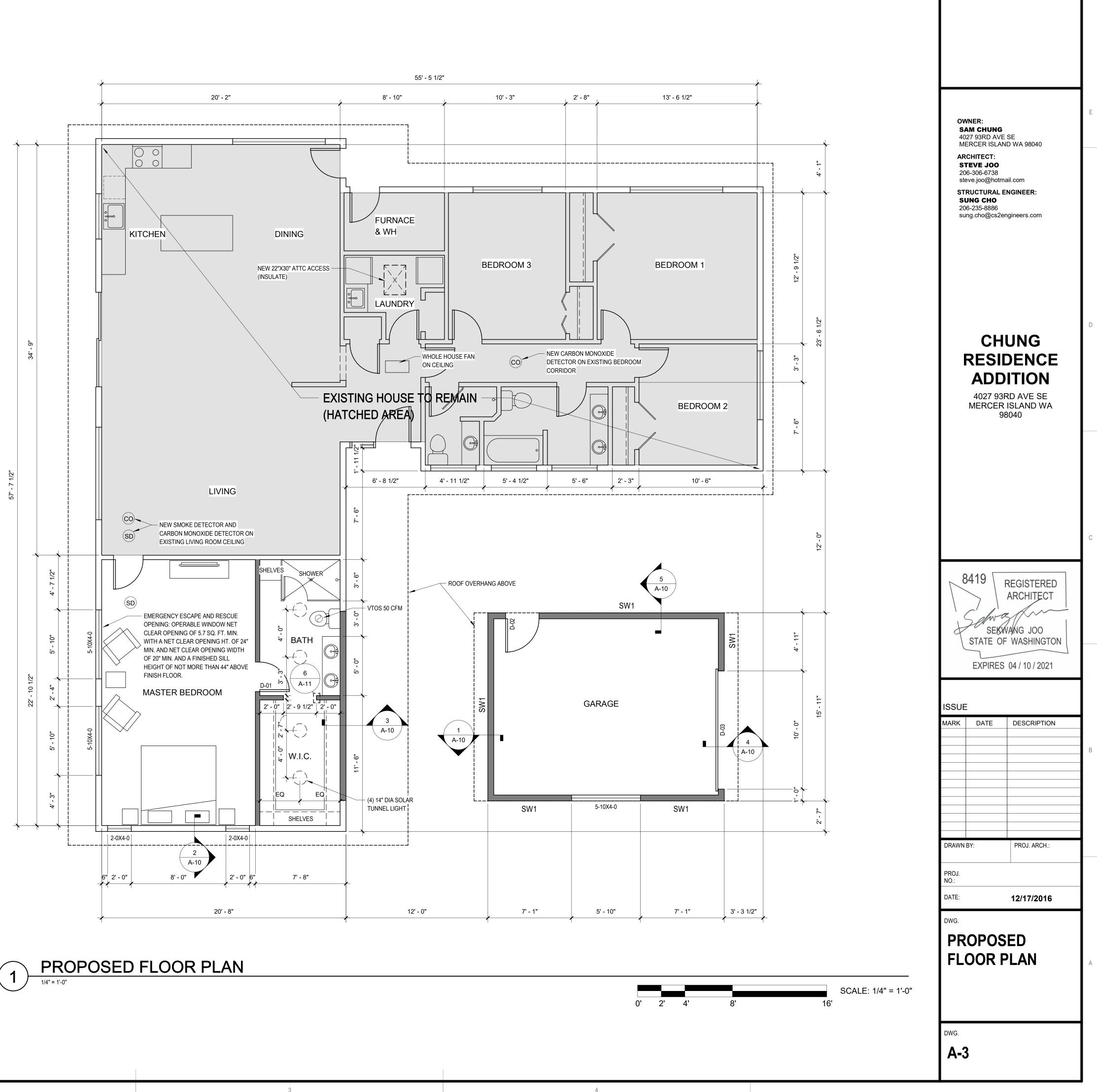
BATHROOM (3)	.25 w.g.	50 CFM
LAUNDRY	.25 w.g.	50 CFM
KITCHEN	.10 w.g.	100 CFM
BEDROOM (4)	.25 w.g.	50 CFM
PROVIDE 150 CFM W.H.I TO CONTROL EXHAUST INTERMITTENT AIR FLO VENTILATION PATH THF	[°] FAN. W AT 50% I	RUN TIME.

DOOR SCHEDULE

DOOR	TYPE	S	IZE	MATERIAL	DE	TAIL	U-VALUE
NUMBER		WIDTH	HEIGHT		HEAD	JAMB	
D-01	SWING / SINGLE / PANEL	2'-6"	6'-8"	WOOD	4/A-14	5/A-14	
D-02	SWING / SINGLE / PANEL	2'-10"	6'-8"	WOOD			
D-03	OVERHEAD GARAGE DOOR	10'-0"	7'-0"	STEEL			

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60

JOISTS & RAFTERS: HF#2 OR BETTER STUDS, PLATES & MISC. FRAMING: HF STUD GRADE 2. STRUCTURAL WOOD PANEL SHEATHING (PLYWOOD) SHALL BE APA RATED SHEATHING WITH EXPOSURE 1 CLASSIFICATION. ROOF SHEATHING SHALL BE 1/2" (NOMINAL) WITH SPAN RATING 24/0. FLOOR SHEATHING SHALL BE 3/4" (NOMINAL) WITH SPAN RATING 40/20. WALL SHEATHING SHALL BE 1/2" (NOMINAL) WITH SPAN RATING 24/0. REFER TO WOOD FRAMING NOTES BELOW FOR TYPICAL NAILING

3. ALL WOOD IN DIRECT CONTACT WITH CONCRETE OR MASONRY SHALL BE PRESSURE- TREATED WITH AN APPROVED PRESERVATIVE OR (2) LAYERS OF ASPHALT IMPREGNATED BUILDING PAPER SHALL BE PROVIDED BETWEEN - UNTREATED WOOD AND CONCRETE OR MASONRY.

STRUCTURAL DESIGN NOTES:

FLOOR LIVE LOAD = 40 PSF

ROOF LIVE LOAD = 25 PSF (SNOW)*

1609 & ASCE 7-10, CHAPTER 26 THRU 31.

WIND IMPORTANCE FACTOR (IW) = 1.0,

BASIC WIND SPEED (V3S) = 110 mph

TOPOGRAPHIC EFFECT (KZT) = 1.0

SEISMIC IMPORTANCE FACTOR (IE) = 1.0,

ROOF TRUSS DEAD LOAD = 10 PSF TOP CHORD + 5 PSF BOTTOM CHORD

3. WIND LOAD: WIND LOADS SHALL BE IN ACCORDANCE WITH THE IBC SECTION

4. SEISMIC LOAD: EARTHQUAKE LOADS SHALL BE IN ACCORDANCE WITH THE

1. FRAMING LUMBER SHALL BE KILN DRIED OR MC-19, AND GRADED AND

WEST COAST LUMBER NO. 16. FURNISH TO THE FOLLOWING MINIMUM

MARKED IN CONFORMANCE WITH W.C.L.B. STANDARD GRADING RULES FOR

BUILDING CODE (SECTION 1613) & ASCE 7-10, CHAPTER 12.

1. LIVE LOAD

2. DEAD LOAD

WIND EXPOSURE "B"

SITE CLASS "D", R = 6.5

SS = 1.398g, S1 = 0.537g,

WOOD:

STANDARDS:

REQUIREMENTS.

SDS = 0.932g, SS1 = 0.537g SEISMIC DESIGN CATEGORY = "D"

BEAMS: DF#1 OR BETTER

4. TIMBER CONNECTORS CALLED OUT BY LETTERS AND NUMBERS SHALL BE "STRONG-TIE" BY SIMPSON COMPANY, AS SPECIFIED IN THEIR LATEST CATALOG. EQUIVALENT DEVICES BY OTHER MANUFACTURERS MAY BE SUBSTITUTED, PROVIDED THEY HAVE ICBO APPROVAL FOR EQUAL OR GREATER LOAD CAPACITIES. PROVIDE NUMBER AND SIZE OF FASTENERS AS SPECIFIED BY MANUFACTURER. CONNECTORS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. WHERE CONNECTOR STRAPS CONNECT TWO MEMBERS, PLACE ONE-HALF OF THE NAILS OR BOLTS IN EACH MEMBER. ALL BOLTS IN WOOD MEMBERS SHALL CONFORM TO ASTM A307. PROVIDE WASHERS UNDER THE HEADS AND NUTS OF ALL BOLTS AND LAG SCREWS BEARING ON WOOD. UNLESS NOTED OTHERWISE, ALL NAILS SHALL BE COMMON. ALL SHIMS SHALL BE SEASONED AND DRIED AND THE SAME GRADE (MINIMUM) AS MEMBERS CONNECTED. ALL JOISTS SHALL BE CONNECTED TO FLUSH BEAMS WITH "U" SERIES JOIST HANGERS.

CAST-IN-PLACE CONCRETE:

1. CONCRETE SHALL BE MIXED, PROPORTIONED, CONVEYED AND PLACED IN ACCORDANCE WITH IBC AND ACI 301. CONCRETE SHALL ATTAIN A 28-DAY STRENGTH OF f'c = 3,500 PSI AND MIX SHALL CONTAIN NOT LESS THAN 5 1/2 SACKS OF CEMENT PER CUBIC YARD AND SHALL BE PROPORTIONED TO PRODUCE A SLUMP OF 3" TO 5".

2. THE MINIMUM AMOUNTS OF CEMENTITIOUS MATERIAL MAY BE CHANGED IF A CONCRETE PERFORMANCE MIX IS SUBMITTED TO THE STRUCTURAL ENGINEER AND THE BUILDING DEPARTMENT FOR APPROVAL TWO WEEKS PRIOR TO PLACING ANY CONCRETE. THE PERFORMANCE MIX SHALL INCLUDE THE AMOUNTS OF CEMENT, FINE AND COARSE AGGREGATE, WATER AND

ADMIXTURES, AS WELL AS THE WATER-CEMENT RATIO, SLUMP, CONCRETE YIELD AND SUBSTANTIATING STRENGTH DATA IN ACCORDANCE WITH ACI 318. REVIEW OF MIX SUBMITTALS BY THE ENGINEER OF RECORD INDICATES ONLY THAT INFORMATION PRESENTED CONFORMS GENERALLY WITH CONTRACT DOCUMENTS. CONTRACTOR OR SUPPLIER MAINTAINS FULL RESPONSIBILITY FOR SPECIFIED PERFORMANCE.

3. SLEEVES, OPENINGS, CONDUIT, AND OTHER EMBEDDED ITEMS NOT SHOWN ON THE STRUCTURAL DRAWINGS SHALL BE APPROVED BY THE PROJECT ARCHITECT/ENGINEER BEFORE POURING. CONDUITS EMBEDDED IN SLABS SHALL NOT BE LARGER IN OUTSIDE DIMENSION THAN ONE THIRD OF THE THICKNESS OF THE SLAB AND SHALL NOT BE SPACED CLOSER THAN THREE DIAMETERS ON CENTER.

4. WHERE NEW CONCRETE IS PLACED AGAINST EXISTING CONCRETE, THE EXISTING CONCRETE SURFACE SHALL BE CLEANED AND ROUGHENED TO A MINIMUM 1/4" AMPLITUDE. PROVIDE 3/4" CHAMFERS ON ALL EXPOSED CONCRETE EDGES, UNLESS NOTED OTHERWISE.

5. VERIFY ALL BLOCKOUTS WITH ARCHITECTURAL, MECHANICAL, ELECTRICAL, AND PLUMBING REQUIREMENTS.

REINFORCING STEEL:

1. REINFORCING STEEL SHALL CONFORM TO ASTM A615, INCLUDING S1, GRADE 60, FOR DEFORMED BARS AND ASTM A185 FOR SMOOTH WELDED WIRE FABRIC (WWF), UNLESS OTHERWISE NOTED. REINFORCING STEEL TO BE WELDED SHALL CONFORM TO ASTM A706. COLUMN SPIRALS SHALL BE PLAIN OR DEFORMED BARS CONFORMING TO ASTM A615, GRADE 60. REINFORCING STEEL SHALL BE SECURELY TIED IN PLACE WITH #16 ANNEALED IRON WIRE.

2. BARS IN SLABS SHALL BE SUPPORTED ON WELL-CURED CONCRETE BLOCKS OR APPROVED METAL CHAIRS, AS SPECIFIED BY THE CRSI MANUAL OF STANDARD PRACTICE, MSP-1. REINFORCING STEEL SHALL BE DETAILED IN ACCORDANCE WITH THE "ACI MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES," ACI 315. SHOP DRAWINGS SHALL INCLUDE ELEVATIONS OF ALL COLUMNS SHOWING BAR LOCATIONS. LAP ALL REINFORCING BARS AT SPLICES 36 DIAMETERS, WITH A MINIMUM LAP OF 18", EXCEPT AS NOTED.

3. CONCRETE PROTECTION (COVER) FOR REINFORCING STEEL SHALL BE AS FOLLOWS, UNLESS NOTED OTHERWISE:

CAST AGAINST EARTH...... 3 IN.

CONCRETE EXPOSED TO EARTH OR WEATHER: #6 AND LARGER 2 IN.

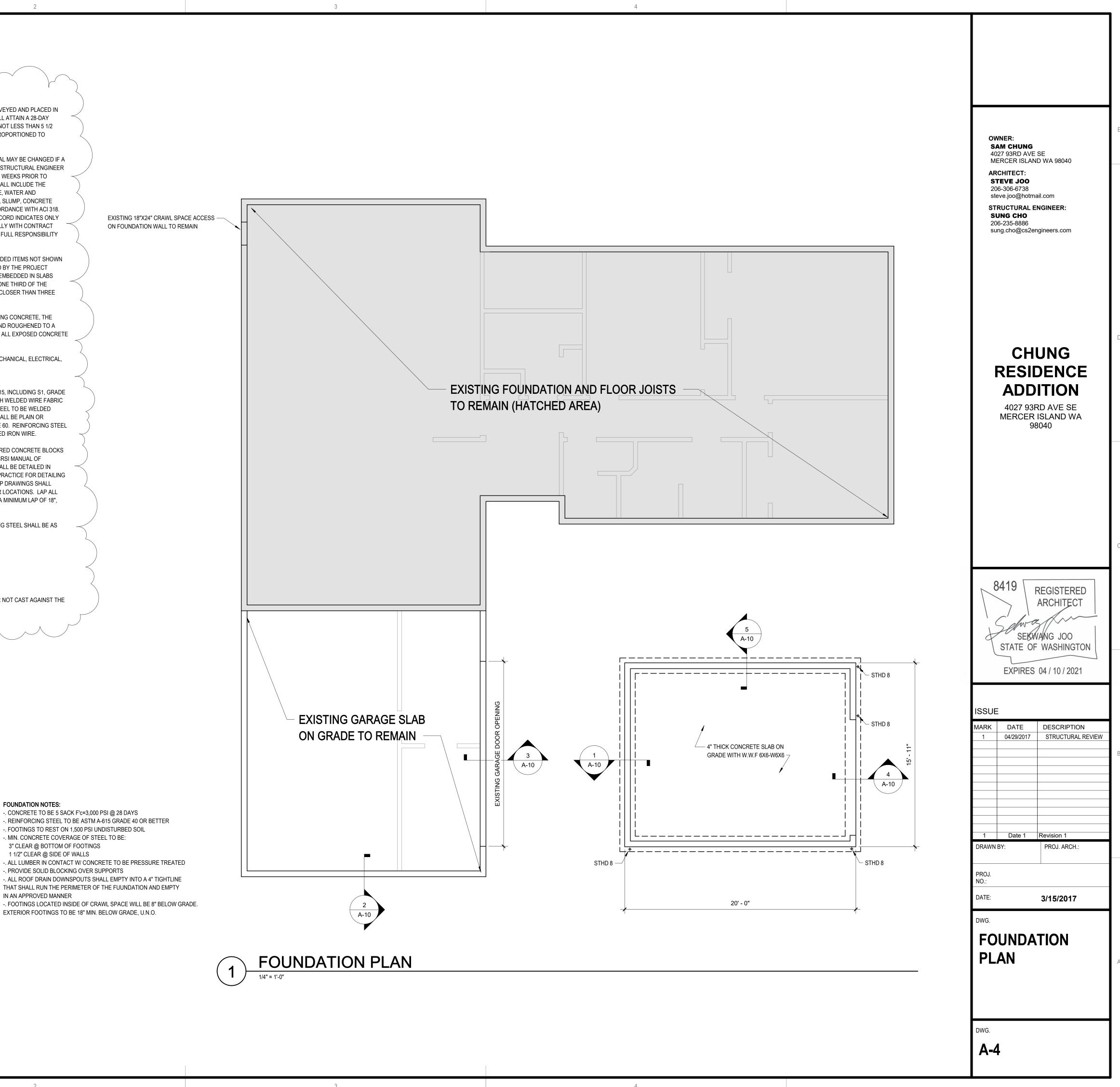
#5 AND SMALLER & WWF 1 1/2 IN.

BEAMS AND COLUMNS 1 1/2 IN.

CONCRETE NOT EXPOSED TO EARTH OR WEATHER OR NOT CAST AGAINST THE GROUND::

FOUNDATION NOTES:

- IN AN APPROVED MANNER





THE ENGINEERING CALCULATIONS. - SHALL HAVE DESIGN DETAILS AND DRAWINGS ON SITE. TRUSS HOLDDOWN: SIMPSON H8 HURRICANE TIE-G90 GALVANIZED AT EACH TRUSS ROOF NOTES: MATERIAL: COMP ROOFING

GENERAL NOTES:

- FIREBLOCK ALL VOIDS.

- SHALL HAVE MANUFACTURER'S STAMP.

TRUSS NOTES:

ATTIC VENTILATION:

IRC SECTION R806.2

PITCH: 5:12 OVERHANG: SEE DIMENSIONS ON PLAN FASCIA: 1"X6" PRIMED AND PAINTED SPRUCE FASCIA BOARD, 1"X8" PRIMED AND PAINTED SPRUCE FASCIA BOARD AT GABLE

- VERIFY IN THE FIELD ALL DIMENSIONS FOR EXISTING CONDITION

- SKYLIGHTS IN HEATED ENVELOP TO BE INSULATED PER WSEC.

- ALL HEADERS TO BE 6X8 DF#2 OR 4X10 DF#2 UNLESS NOTED OTHERWISE. - ALL LUMBER EXPOSED TO WEATHER TO BE PRESSURE TREATED OR CEDAR.

- SHALL BE INSTALLED AND BRACED PER MANUFACTURER'S INSTRUCTIONS. - WILL NOT BE FIELD ALTERED WITHOUT BUILDING DEPARTMENT APPROVAL OF

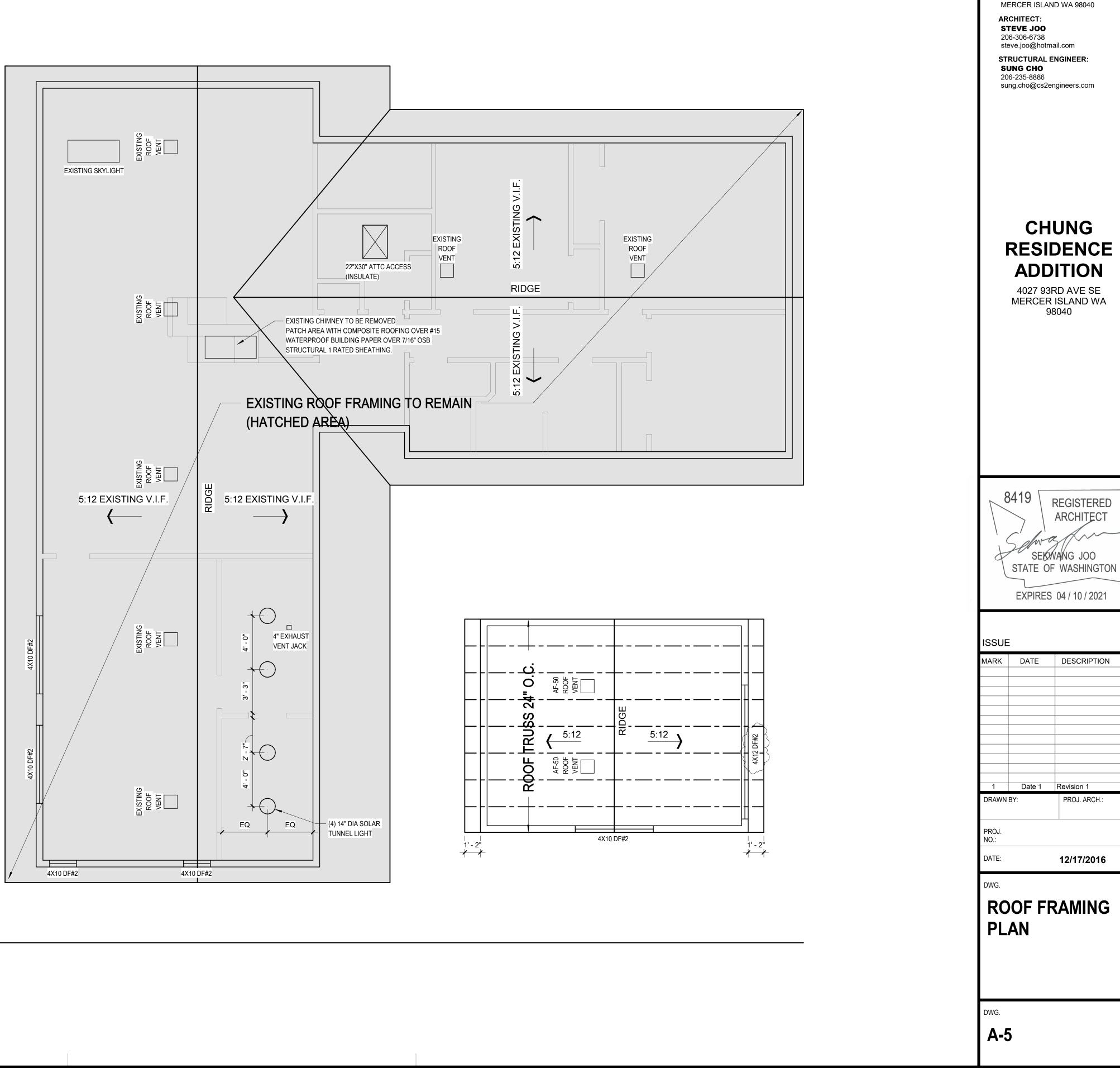
- MIN. 22"X30" ATTIC ACCESS W/ 36" MIN. HEAD ROOM. INSULATE AND WEATHERSTRIP.

EXISTING HOUSE: EXITING ATTIC VENTS TO REMAIN. NEW GARAGE: 356 SQ. FT.(ATTIC SPACE) X 144/300 = 170.88 SQ. IN.(REQUIRED NET AREA) PROVIDE (10) 7.85 SQ. IN. VENT BLOCKS = 78.5 SQ. IN. PROVIDE (2) AF-50 ROOF JACKS = 100 SQ. IN.



ROOF FRAMING PLAN 1

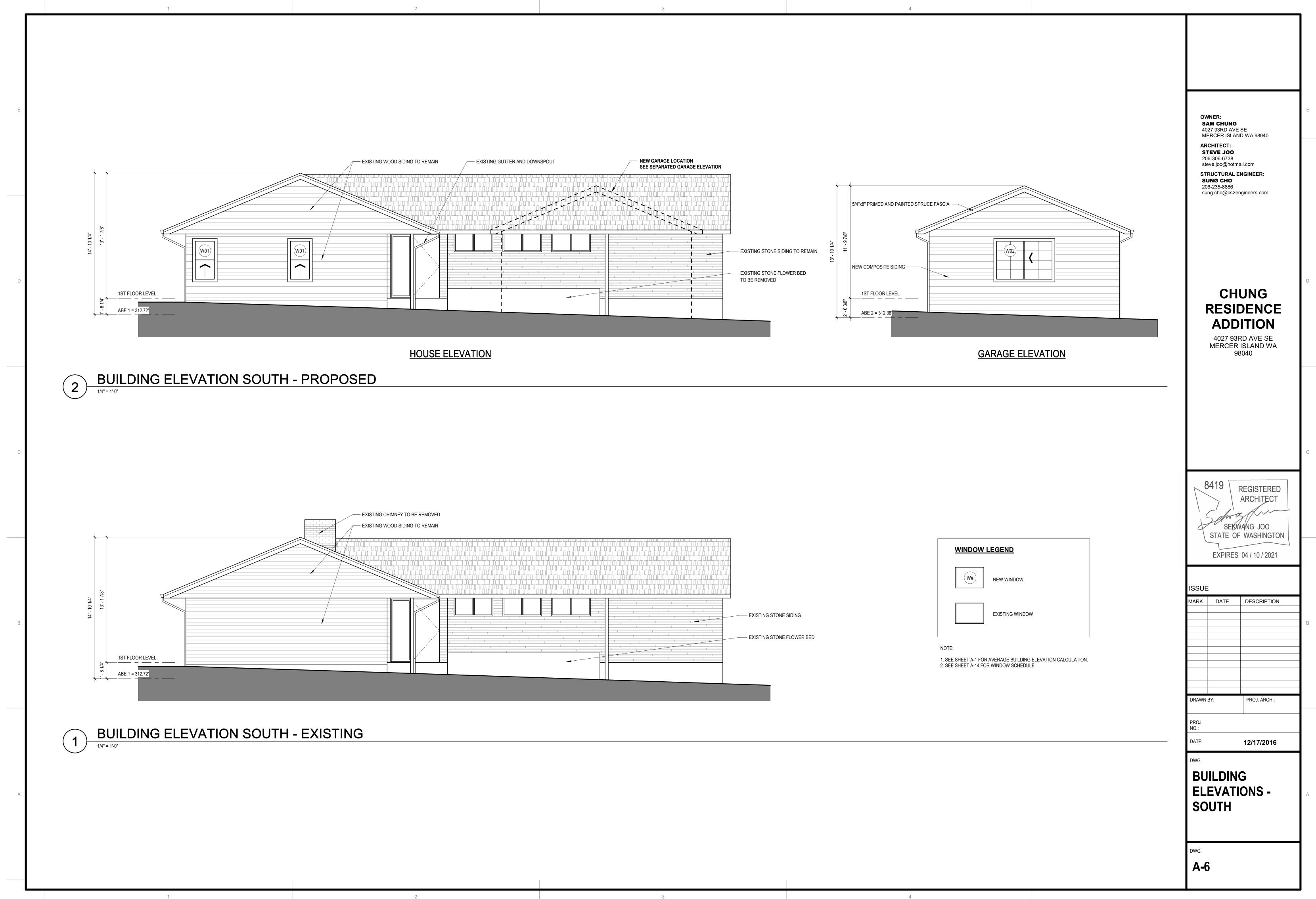
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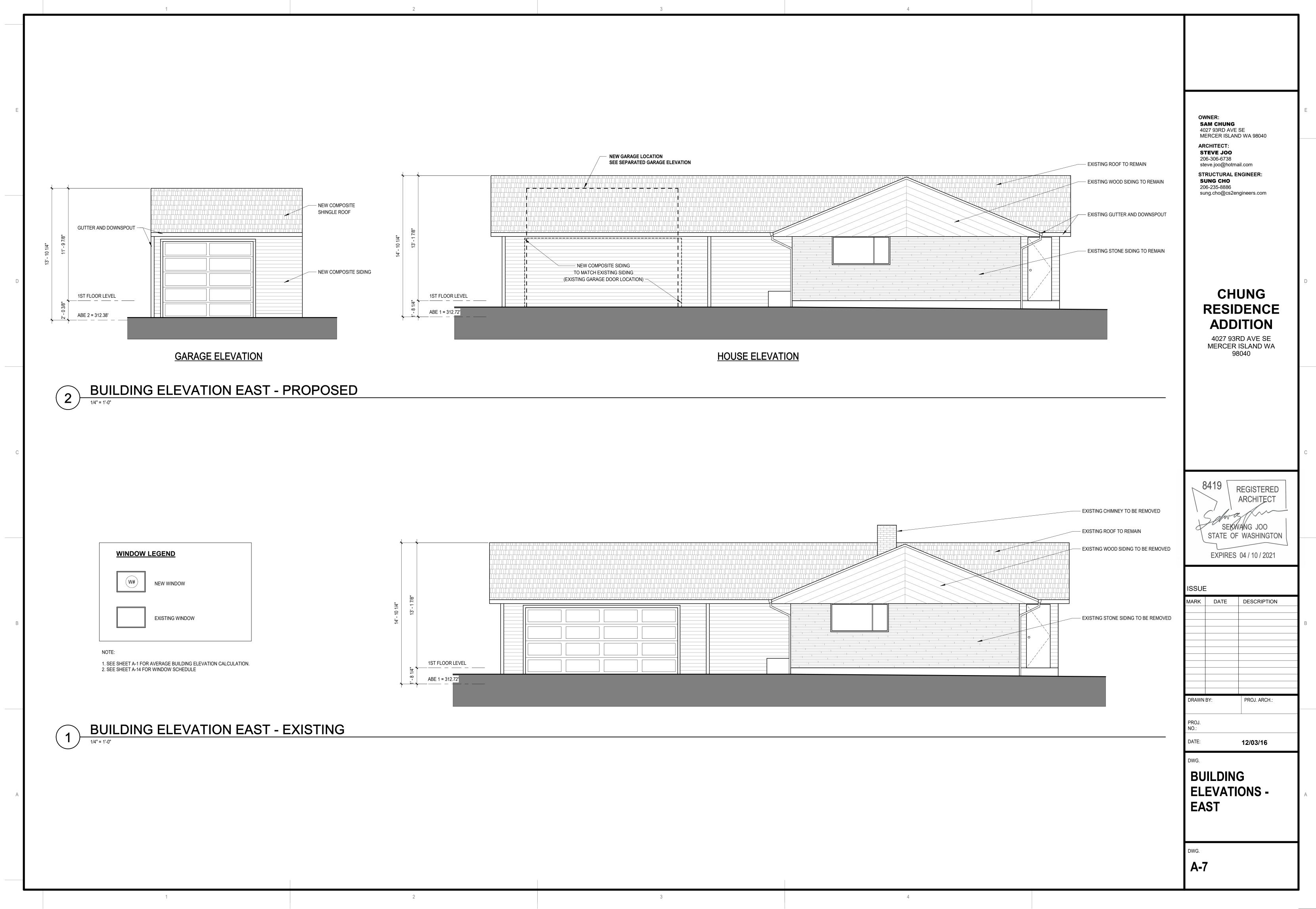
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OWNER:

SAM CHUNG 4027 93RD AVE SE

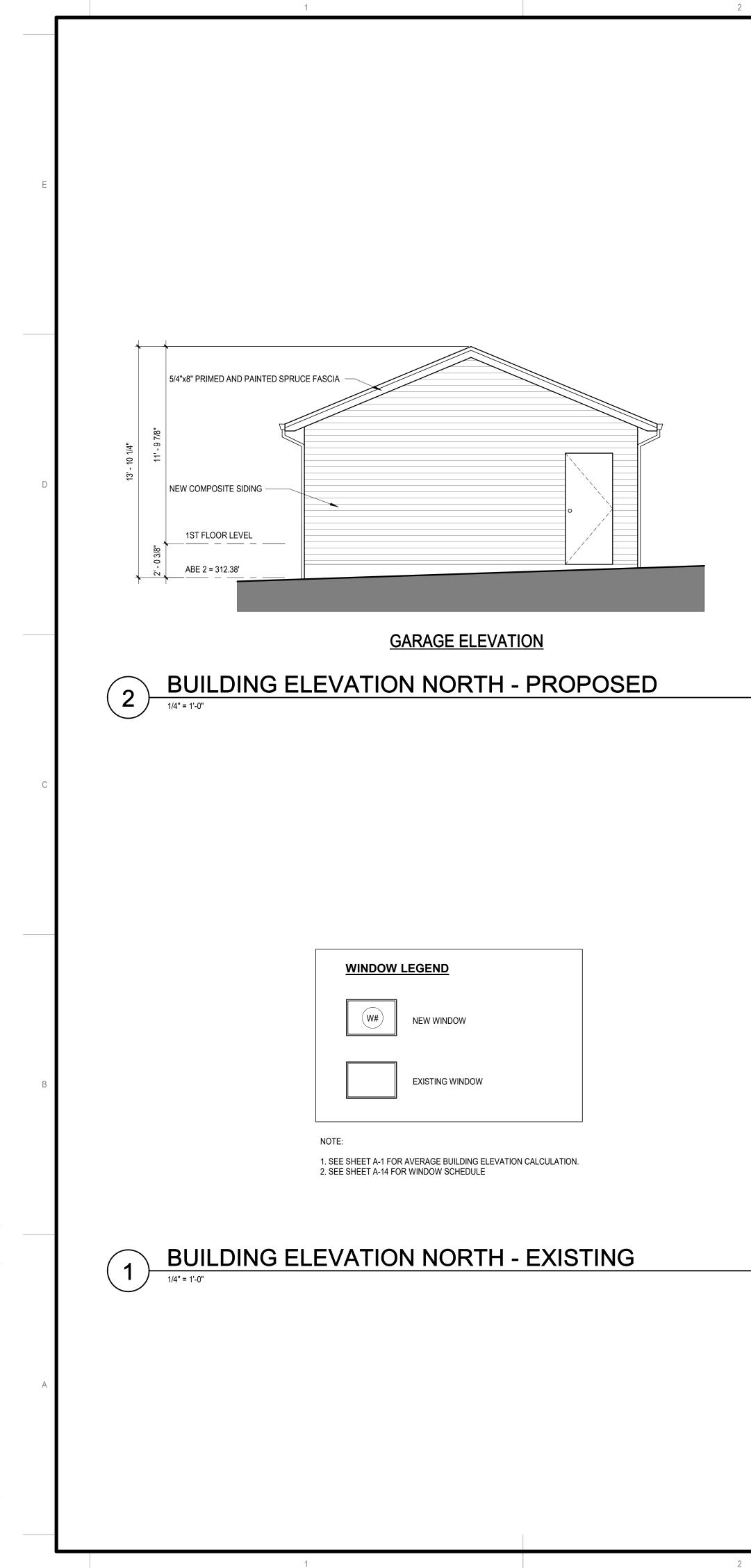


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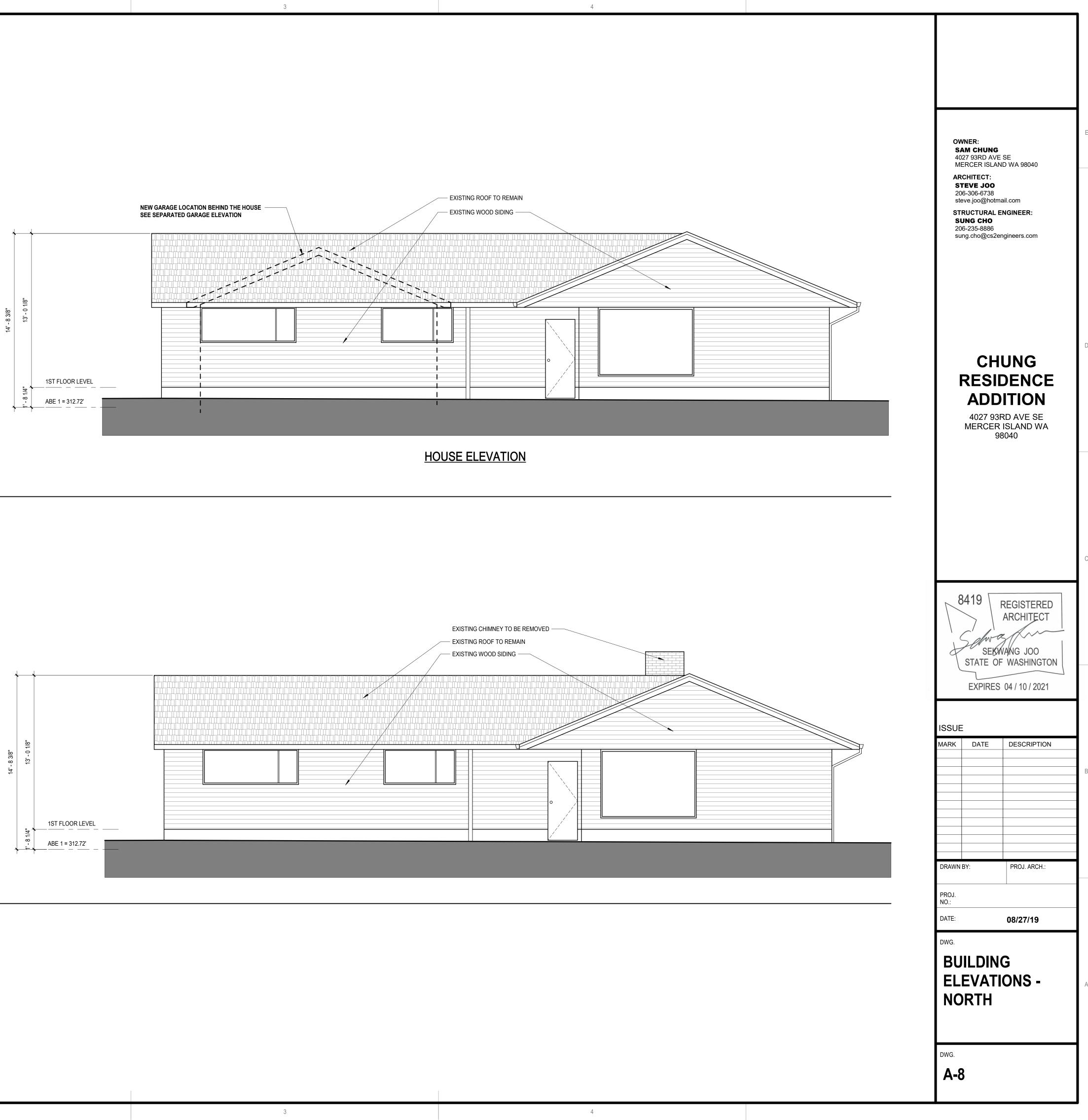


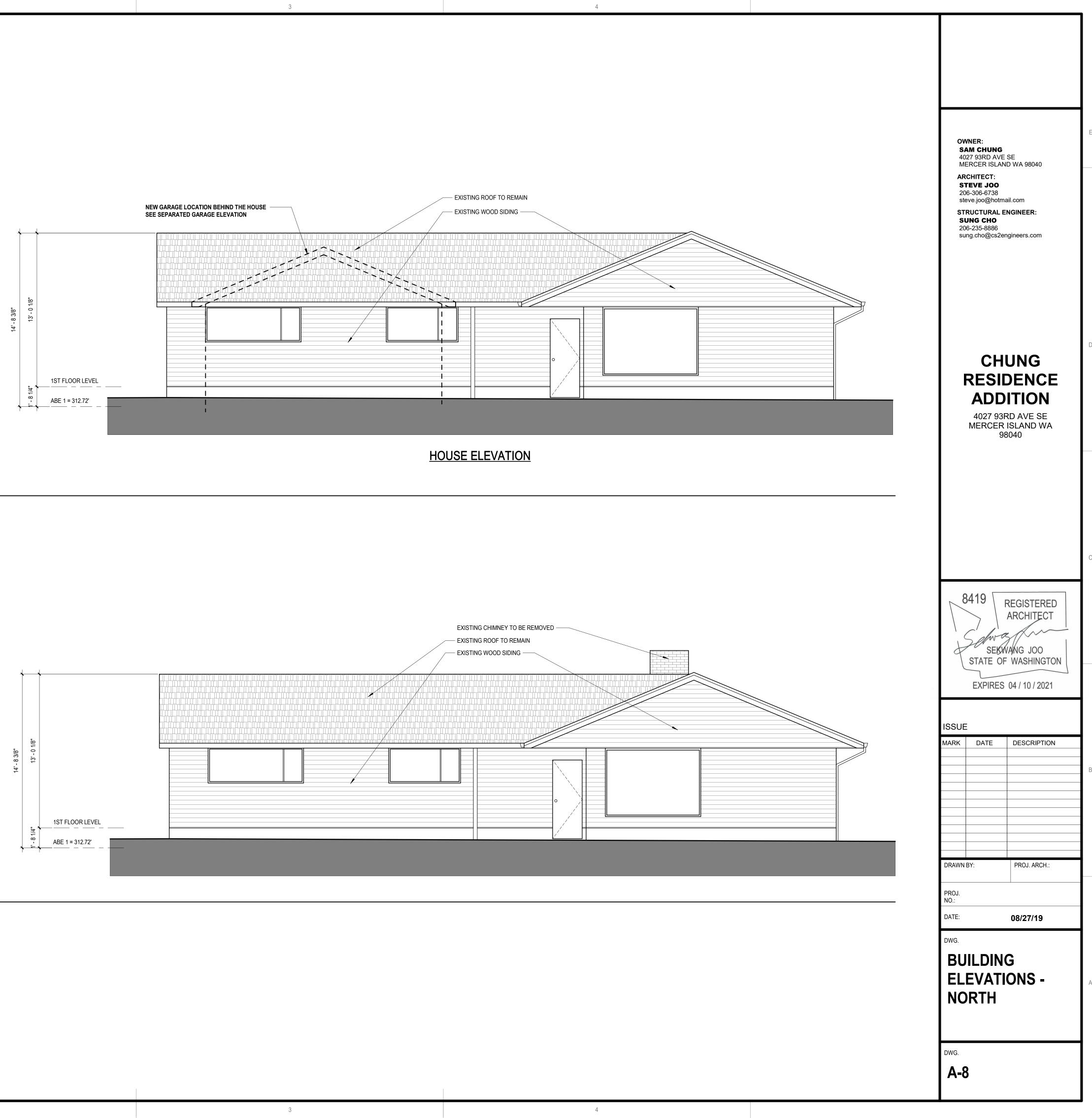
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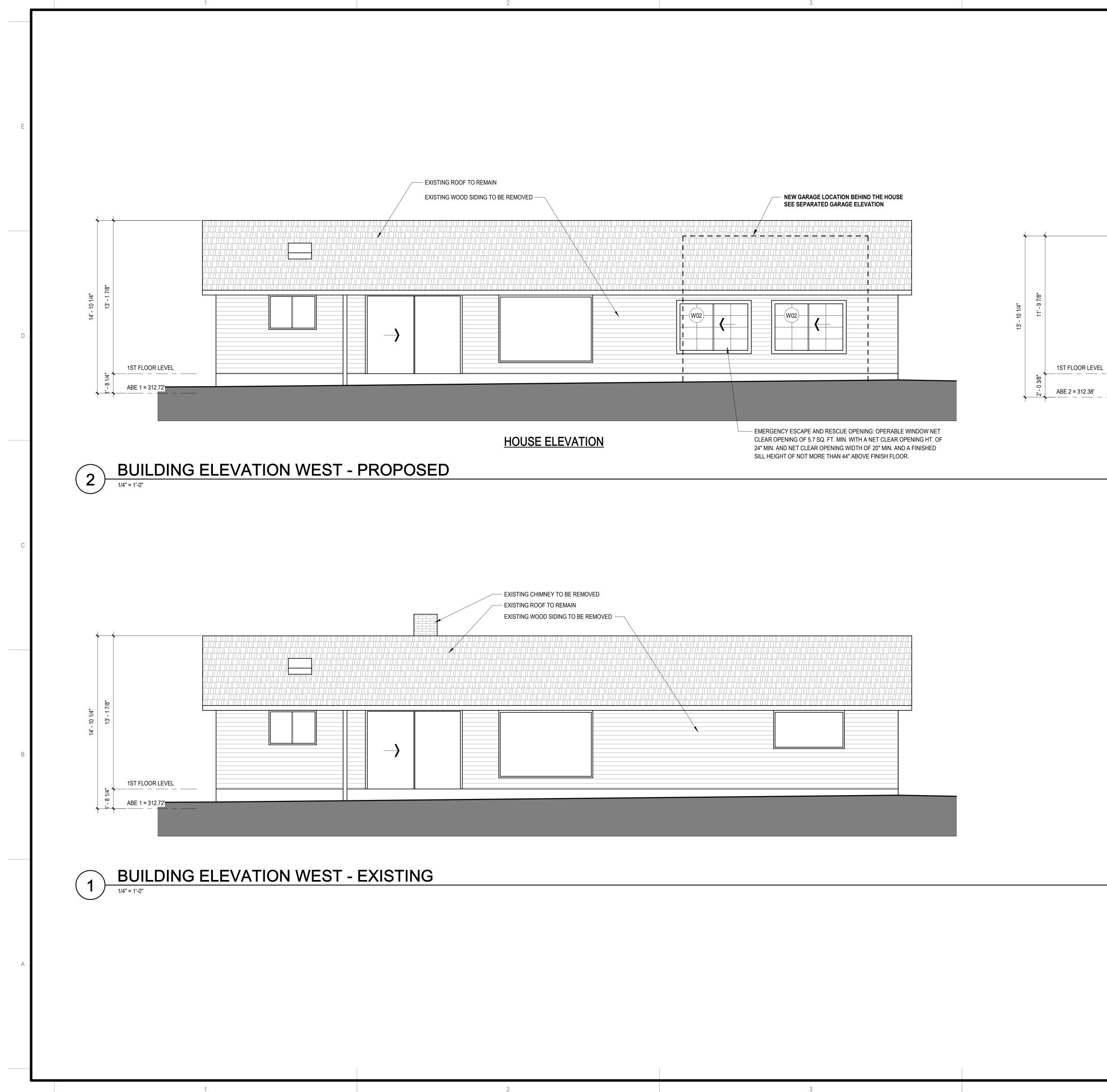
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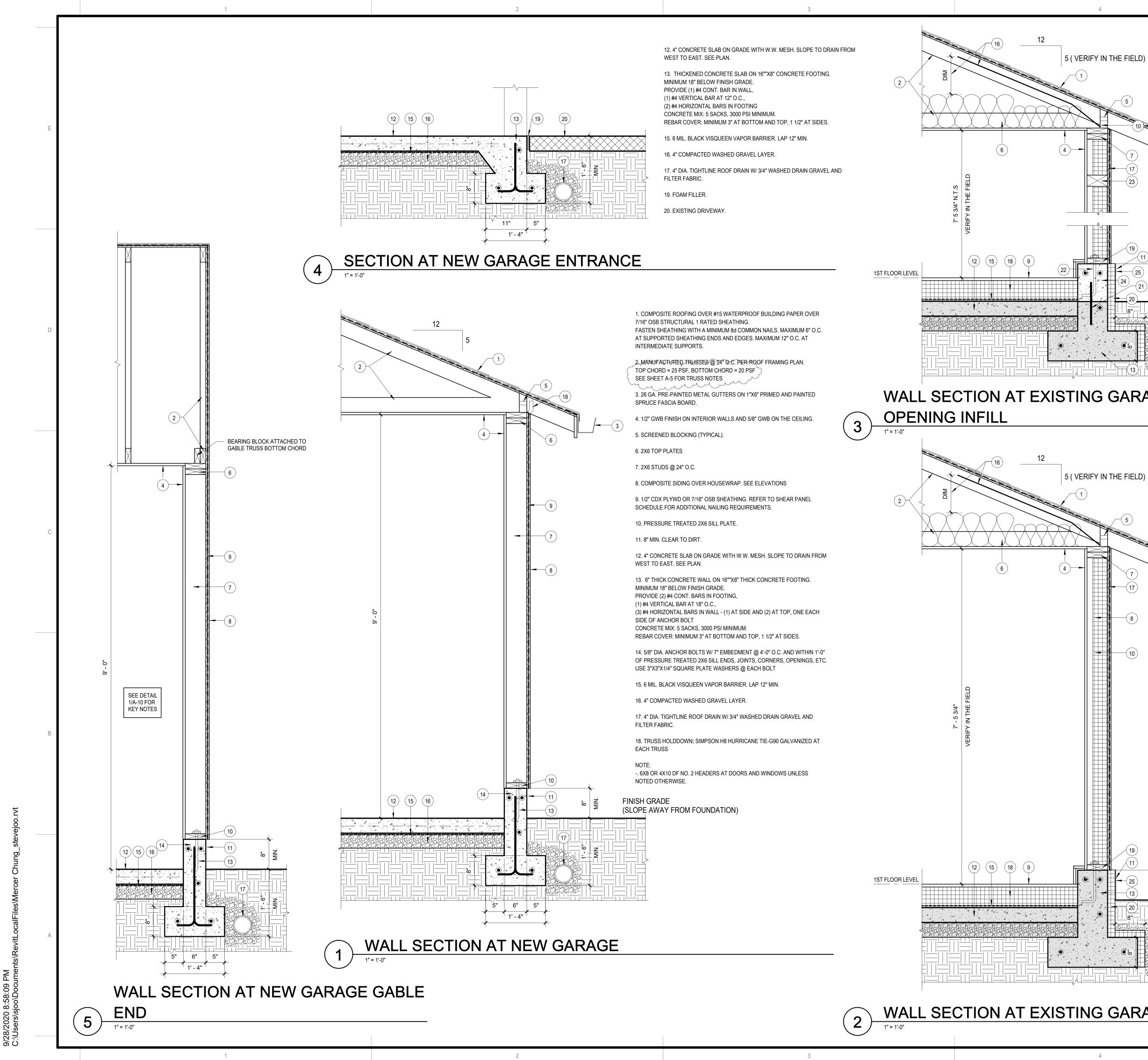






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<image/>	ARCHITECT: STEVE JOO 206-306-673: steve.joo@hd STRUCTURA SUNG CHO 206-235-8886 sung.cho@cs Sung.cho@cs CI RES AD 4027 S	VE SE AND WA 98040 3 otmail.com L ENGINEER:	
WINDOW LEGEND W# NEW WINDOW EXISTING WINDOW	STATE	REGISTERED ARCHITECT KWANG JOO OF WASHINGTON ES 04 / 10 / 2021	E
NOTE: 1. SEE SHEET A.14 FOR WINDOW SCHEDULE SEE SHEET A.14 FOR WINDOW SCHEDULE	DRAWN BY: PROJ. NO.: DATE: DWG. BUILDI ELEVA WEST DWG. DWG.	PROJ. ARCH.: 08/27/19	ŀ



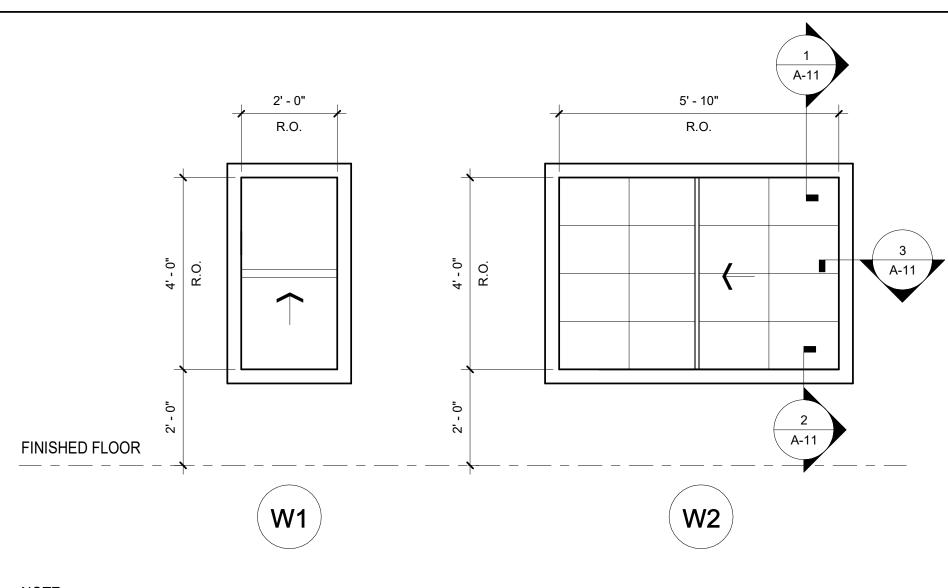
WALL SECTION AT EXISTIN 1" = 1'-0"

)	SET-XP EPOXY 22. NEW 5/8" DIA. ANCHOR BOLTS W/ 7" EMBEDMENT @ 4'-0" O.C. AND WITHIN				
5	1'-0" OF PRESSURE TREATED 2X4 SILL ENDS, JOINTS, CORNERS, OPENINGS, ETC.				
	USE 3"X3"X1/4" SQUARE PLATE WASHERS @ EACH BOLT				Е
	23. EXISTING GARAGE DOOR HEADER 24. NEW 8" THICK AND 8" TALL CONCRETE WALL ON EXISTING CONCRETE		/NER: A M CHUNG		E
	FOOTING. PROVIDE	40	27 93RD AVE ERCER ISLAN		
	(1) #4 VERTICAL BAR AT 18" O.C., (2) #4 HORIZONTAL BARS IN WALL - (2) AT TOP, ONE EACH SIDE OF ANCHOR		CHITECT: EVE JOO		
	BOLT CONCRETE MIX: 5 SACKS, 3000 PSI MINIMUM.	20	6-306-6738 eve.joo@hotm	ail.com	
	REBAR COVER: MINIMUM 3" AT BOTTOM AND TOP, 1 1/2" AT SIDES.		RUCTURAL E JNG CHO	NGINEER:	
	25. PAINTED METAL FLASHING OVER RIGID INSULATION.	20	6-235-8886 ng.cho@cs2e	ngineers.com	
	SEE DETAIL 2/A-10 FOR OTHER NOTES NEW 6X8 OR 4X10 DF NO. 2 HEADERS AT DOORS AND WINDOWS UNLESS NOTED OTHERWISE.				
	H GRADE				
	PE AWAY FROM FOUNDATION)				
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				ITION	
NG GARAGE DOOF	2			RD AVE SE	
	•			ISLAND WA 8040	
			Ū.		
	1. EXISTING COMPOSITE ROOFING OVER OSB SHEATHING.				
RIFY IN THE FIELD)	2. EXISTING TRUSSES.				
)	3. EXISTING METAL GUTTERS ON FASCIA BOARD.				
(5)	4. NEW 1/2" GWB FINISH ON INTERIOR WALLS AND 5/8" GWB ON THE CEILING.				
	5. EXISTING SCREENED BLOCKING (TYPICAL).				С
	6. NEW R-49 BATT INSULATION AT CEILINGS. INSURE 1" CONTINUOUS AIR SPACE OVER BATTS, VENTS EAVES, AND RIDGES.				
	7. EXISTING 2X4 TOP PLATES	R	8419		
	8. EXISTING 2X4 STUDS	N		REGISTERED ARCHITECT	
	9. NEW 3/4" UL GRADE T&G PLYWOOD.		71		
	10. NEW R-21 RIGID INSULATION.	t	SEK	VANG JOO	
	11. 8" MIN. CLEAR TO DIRT.		-	WASHINGTON	
	12. EXISTING CONCRETE SLAB ON GRADE (GRAY HATCHED).		EXPIRES	04 / 10 / 2021	
	13. EXISTING CONCRETE FOUNDATION WALL ON CONCRETE FOOTING (GRAY HATCHED).				
	14. EXISTING ROOF DRAIN.	ISSUE	:		
	15. NEW 6 MIL. BLACK VISQUEEN VAPOR BARRIER. LAP 12" MIN.	MARK	DATE	DESCRIPTION	1
	16. NEW WIND BAFFLE BETWEEN EACH TRUSS SPACE. (12" ABOVE BLOWN AND 6" ABOVE BATT)	1	04/29/2017	STRUCTURAL REVIEW	1
	17. EXISTING WOOD SIDING.				В
	18. NEW PRESSURE TREATED 2X8 AT 16" O.C. W/ RIGID INSULATION IN				
	BETWEEN, R-28 MINIMUM AT THE HIGHEST ELEVATION OF EXISTING SLAB, R-42 AT THE LOWEST. CUT 2X8 AND MATCH SLOPE OF EXISTING SLAB FOR				
	LEVELING PLYWOOD FLOOR.				
	19. EXISTING PRESSURE TREATED 2X4 SILL PLATE. 20. NEW R-10 REGID INSULATION, 24" IN LENGTH MINIMUM	1	Date 1	Revision 1	
	25. PAINTED METAL FLASHING OVER RIGID INSULATION.	DRAWN		PROJ. ARCH.:	1
	NOTE:				╞
	NEW 6X8 OR 4X10 DF NO. 2 HEADERS AT DOORS AND WINDOWS UNLESS NOTED OTHERWISE.	PROJ. NO.:			
		DATE:		12/17/2016	
	H GRADE PE AWAY FROM FOUNDATION)	DWG.			1
		WALL SECTIONS			
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NG GARAGE			DWG. A-10		
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19. NEW PRESSURE TREATED 2X4 SILL PLATE.

21. NEW #4 BAR AT 12" O.C.. EMBED 6" INTO EXISTING FOOTING. STRONG-TIE

WINDOW SCHEDULE



NOTE:

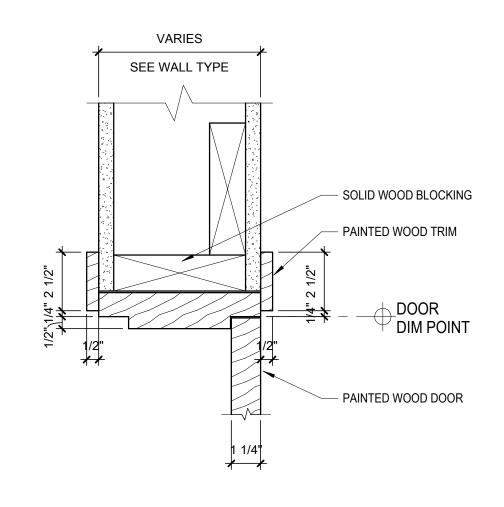
1. VINYL FRAME WINDOWS, JELD-WEN V-4500 SERIES, INSULATED LOW-E CLEAR FLOAT, DOUBLE PANE, AIR FILLED, FRAME COLOR: WHITE, TARGET U-VALUE < 0.3 SEE 5, 6 & 7/ A-11 FOR WINDOW DETAILS

2. TEMPERED GLASS REQUIREMENT: PER IRC R308.4.3 GLAZING IN WINDOWS, GLAZING IN AN INDIVIDUAL FIXED OR OPERABLE PANEL THAT MEETS ALL OF THE FOLLOWING CONDITIONS SHALL BE CONSIDERED TO BE A HAZARDOUS LOCATION. -. THE EXPOSED AREA OF AN INDIVIDUAL PANE IS LARGER THAN 9 SQUARE FEET, AND

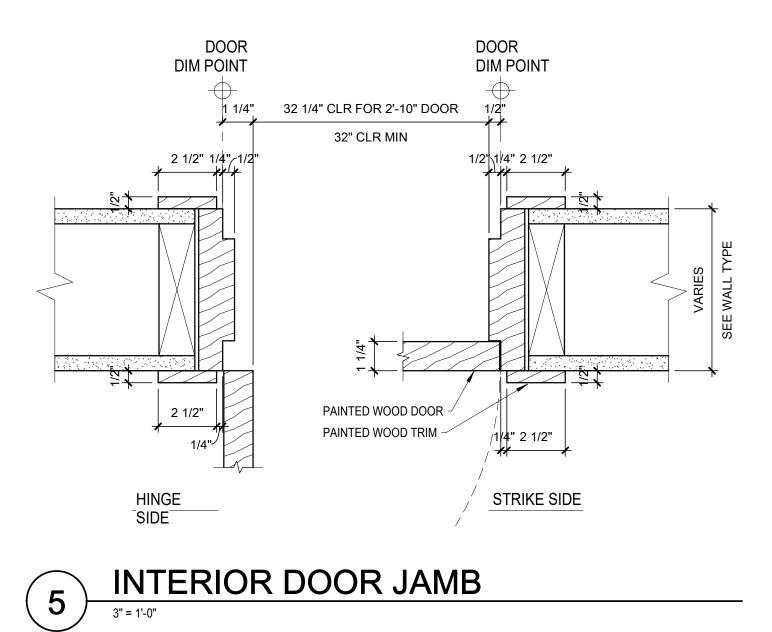
-. THE BOTTOM EDGE OF THE GLAZING IS LESS THAN 18 INCHES ABOVE THE FLOOR, AND -. THE TOP EDGE OF THE GLAZING IS MORE THAN 36 INCHES ABOVE THE FLOOR, AND

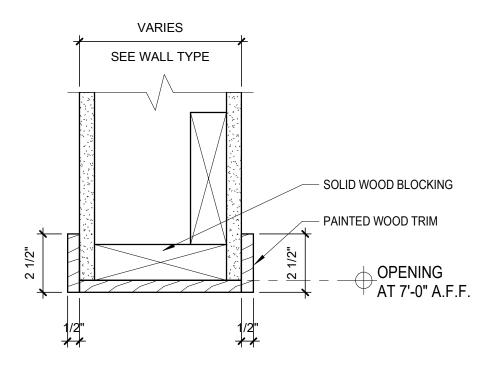
-. ONE OR MORE WALKING SURFACES ARE WITHIN 36 INCHES, MEASURED HORIZONTALLY AND IN A STRAIGHT LINE



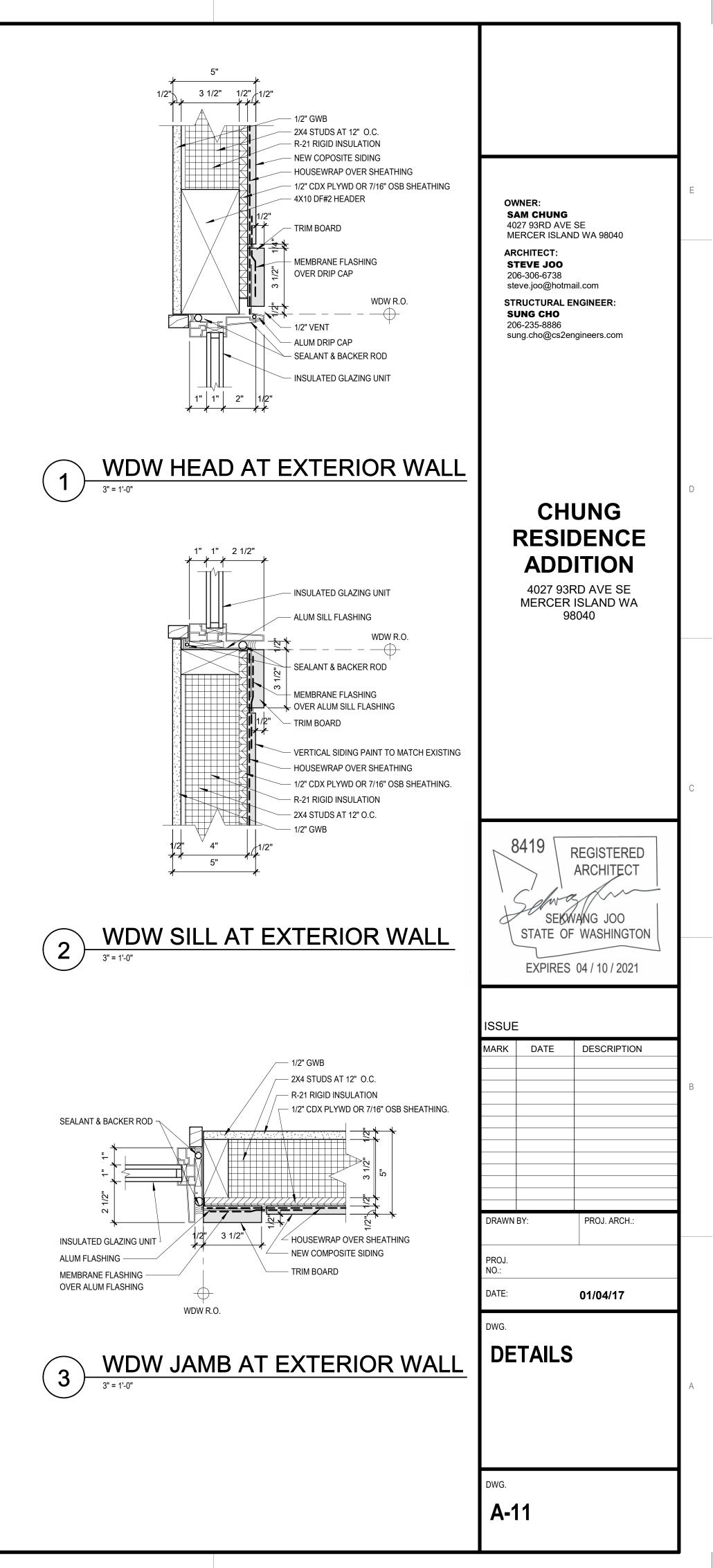












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